03 - STRUCTURES INDEX OF DRAWINGS

DRAWING NUMBER	DRAWING TITLE
S-1	BRIDGE DESIGN INDEX OF DRAWINGS
S-2	BRIDGE INFORMATION TABLE 1
S-3	BRIDGE INFORMATION TABLE 2
S-4	BRIDGE INFORMATION TABLE 3
S-5	ASPHALTIC PLUG EXPANSION JOINT SYSTEM DETAILS
S-6	ASPHALTIC PLUG EXPANSION JOINT SYSTEM DETAILS
S-7	PREFORMED SILICONE JOINT SEAL DETAILS
S-8	ASPHALTIC PLUG EXPANSION JOINT SYSTEM DETAILS
S-9	STAGE CONSTRUCTION JOINT IN PAVEMENT CUTOUT
S-10	PARAPET JOINT DETAILS 1
S-11	PARAPET JOINT DETAILS 2A
S-12	PARAPET JOINT DETAILS 2B
S-13	PARAPET JOINT DETAILS 3
S-14	PARAPET JOINT DETAILS 4
S-15	PARAPET JOINT DETAILS 5

GENERAL NOTES

- 1. FOR BRIDGE LOCATIONS SEE HIGHWAY PLANS.
- FOR ALL BRIDGE PARAPET JOINT DETAILS SEE DRAWING NO. S-9, DRAWING TITLE: STAGE CONSTRUCTION JOINT IN PAVEMENT CUTOUT.
- THE DEPTH OF PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM IS ESTIMATED TO BE ON AVERAGE, $\frac{3}{4}$ ".
- USE ASPHALTIC PLUG JOINT SYSTEM FOR ABUTMENTS NOS. 1 AND 2 OF BRIDGE NO. 01770
- DUE TO THE DIFFERENTIAL DEPTH BETWEEN ROADWAY AND BRIDGE ASPHALT MILLING AND PRIOR TO MILLING OPERATIONS THE CONTRACTOR SHALL VERIFY THE ENDS OF CONCRETE BRIDGE DECKS AND BACKWALL AND ADJUST MILLING OPERATIONS TO PROTECT THE CONCRETE FROM DAMAGE.

SHEET NO. Plotted Date: 10/17/2013

JOINT THREATMENT SHALL BE PERFORMED ONLY AT SOUTHBOUND FOR THE BRIDGE NO. 01639.

PREFORMED SILICONE JOINT SEAL NOTES

- PRIOR TO INSTALLING THE SILICONE SEALANT, CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR UNDER THE ITEM "PREFORMED SILICONE JOINT SEALING SYSTEM".
- 2. ALL PREFORMED SILICONE JOINT SEAL SHALL BE SILICOFLEX SF-400 WITH THE EXCEPTION OF THE FOLLOWING JOINTS:

BRIDGE NO. 01770: PIER NOS. 1 AND 2

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.
TRANSPORTATION PRINCIPAL ENGINEER

GENERAL NOTES

- SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 816 (2004), SUPPLEMENTAL SPECIFICATIONS DATED JULY 2013 AND SPECIAL
- DESIGN SPECIFICATIONS: AASHTO LFRD BRIDGE DESIGN SPECIFICATIONS CUSTOMARY U.S. UNITS 6TH EDITION - 2012, INCLUDING 2013 INTERM REVISIONS AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003)
- REINFORCING BARS: REINFORCING BARS IN ALL CONCRETE RECONSTRUCTIONS SHALL CONFORM TO ASTM A615, GRADE 60, AND BE GALVANIZED IN ACCORDANCE WITH ASTM A767, CLASS 1.

ASPHALTIC PLUG EXPANSION JOINT SYSTEM NOTES

- A BRIDGING PLATE SHALL BE USED TO SPAN THE GAP BETWEEN TWO DECK ENDS OR THE JOINT BETWEEN A DECK END AND A CONCRETE BACKWALL.
- DISCONTINUE THE INSTALLATION OF THE BACKER ROD, BRIDGING PLATE AND LOCATING PIN WHERE THE APPROACH SLAB IS DIS-CONTINUED (TYPICALLY IN THE ROADWAY SHOULDERS). SEE ASPHALTIC PLUG EXPANSION JOINT SYSTEM SPECIAL PROVISION.
- NEW STEEL BRIDGING PLATES SHALL HAVE A MINIMUM THICKNESS OF $\frac{1}{4}$ ". FOR JOINT OPENINGS THAT EXCEED 3" A $\frac{3}{8}$ " THICK BY 12" WIDE PLATE WILL BE REQUIRED.
- 4. NO BRIDGING PLATE SHALL BE USED AT THE FOLLOWING LOCATIONS:
 - A. JOINT BETWEEN A DECK END AND A CONCRETE APPROACH PAVEMENT
 - B. WHERE A BRIDGE DECK END MEETS A BITUMINOUS APPROACH PAVEMENT
- SAW-CUTS MADE 3' EACH SIDE OF CENTERLINE OF JOINT WILL BE PAID AS "CUT BITUMINOUS CONCRETE PAVEMENT".
- 6. THE REMOVAL OF ALL EXISTING JOINT SYSTEMS AND BITUMINOUS CONCRETE WITHIN THE LIMITS SHOWN TO BE INCLUDED FOR PAY-MENT UNDER THE ITEM "REMOVAL OF HMA WEARING SURFACE".
- INSTALLATION OF MEMBRANE WITHIN THE LIMITS SHOWN TO BE PAID UNDER THE ITEM, "MEMBRANE WATERPROOFING (SHEET) (TORCH-APPLIED)"
- CRACK SEALANT PLACED ALONG VERTICAL FACES OF THE SAW-CUT PAVEMENT TO BE PAID UNDER THE ITEM, "JOINT AND CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT".
- THE FURNISHING AND PLACING OF HMA S0.375 TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "HMA S0.375".
- 10. SAW-CUTTING AND REMOVAL OF PAVEMENT FOR JOINT INSTALLATION TO BE INCLUDED UNDER FOR PAYMENT THE ITEM, "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
- 11. CLOSED CELL BACKER ROD DIAMETER SHALL BE DETERMINED AFTER MEASURING THE JOINT OPENING. THE ROD SHALL BE 25% LARGER THAN THE JOINT OPENING.
- 12. ASPHALTIC PLUG EXPANSION JOINT SYSTEMS MAY BE INSTALLED ONLY WITHIN THE TEMPERATURE RANGE SPECIFIED IN THE SPECIAL PROVISION "ASPHALTIC PLUG EXPANSION JOINT SYSTEM". REFERENCE THE RANGE OF THERMAL MOVEMENT FOR THE SELECTED JOINT PRODUCT IN THE TABLE FOR "ALLOWABLE BRIDGE SUPERSTRUCTURE SURFACE TEMPERATURE RANGE" IN THE SPECIAL PROVISION.

-	-	-	_	THE INFORMATION, INCLUDING ESTIMATED
_	-	-	-	QUANTITIES OF WORK, SHOWN ON THESI SHEETS IS BASED ON LIMITED
_	-	-	-	INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE
-	-	-	-	THE CONDITIONS OF ACTUAL QUANTITIES
_	-	-	_	OF WORK WHICH WILL BE REQUIRED.
-	-	-	-	

REVISION DESCRIPTION

REV. DATE

SCALE AS NOTED

STATE OF CONNECTICUT **DEPARTMENT OF TRANSPORTATION**

Filename: ...\SB_MSH_S-1_BRIDGE_DESIGN_INDEX_OF_DRAWINGS.dgn

OFFICE OF ENGINEERING

PAVEMENT PRESERVATION ON I-95

TOWN OF GROTON BRIDGE DESIGN

SHEET NO. INDEX OF DRAWINGS 04.01

58-329

S-1

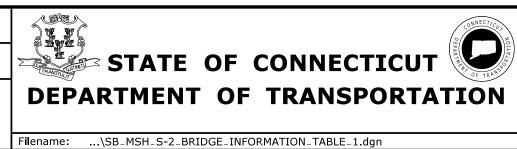
	В	RIDGE INFO	RMATION	FOR THE INSTALLATION OF BR	DGE DECK	JOINTS (TOV	VN OF GRO	TON)					
BRIDGE NO. BRIDGE DATA								BRI	BRIDGE JOINT LOCATIONS				
	FINE MILLING	HMA SO.5	MILE	LOCATION	NO. OF TRAVE	L CURB TO CURB	SKEW	ABUTMENT	ABUTMENT				
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2				
01769	1.5"	2"	94.93	I-95 SOUTHBOUND OVER I-95 RAMP 313,	4	80.0	34.0						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.82				
DECK JOINT DETAIL								B/S-6	B/S-6				
PARAPET JOINT DETAIL								F/S-10	F/S-10				
	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVE	* LCURB TO CURB	** SKEW	ABUTMENT	PIER	PIER	ABUTMENT		
BRIDGE NO.	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2		
01639	1"	2"	94.80	I-95 RAMP 313 OVER AMTRAK	2	35	19						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON - SOUTHBOUND ONLY				0.00	0.85	0.99	0.37		
DECK JOINT DETAIL								B/S-6	N/A	N/A	B/S-6		
PARAPET JOINT DETAIL								Q/S-14	N/A	N/A	Q/S-14		
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVE	* LCURB TO CURB	** SKEW	ABUTMENT	ABUTMENT				
	DEPTH (IN)	DEPTH (IN)	POINT	20 0/11 20 11	LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2				
02445	1.5"	2"	94.93	I-95 NOURTHBOUND OVER I-95 RAMP 313		148.0	3.0	1	NOT 2				
MAXIMUM THERMAL EXPANSION (IN.)			54.55	GROTON	,	140.0	3.0	0.00	0.87				
DECK JOINT DETAIL				GROTON				B/S-6	B/S-6				
PARAPET JOINT DETAIL								0/S-13	0/S-13				
	ETNE MTI I TNO	LIMA CO E	MILE	LOCATION	NO OF TRAVE	*	**			DIED	ADUTMENT		
BRIDGE NO.	FINE MILLING			LOCATION		L CURB TO CURB		ABUTMENT	PIER	PIER	ABUTMENT		
01770	DEPTH (IN)	DEPTH (IN)	POINT	I OF CD OVED DOUTE 101 O I OF TDOOR	LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2		
01770	1.5"	2"	95.19	I-95 SB OVER ROUTE 184 & I-95 TR827,	3	50.0	57.0						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.65	1.01	0.99		
DECK JOINT DETAIL								E/S-8	C/S-7	C/S-7	E/S-8		
PARAPET JOINT DETAIL						* LCURB TO CURB	* *	H/S-11&S-12	S/S-15	S/S-15	H/S-11&S-12		
BRIDGE NO.	FINE MILLING		MILE	LOCATION				ABUTMENT	PIER	PIER	ABUTMENT		
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2		
01771	1.5"	2"	95.30	I-95 SB OVER ROUTE 12,	3	50.0	44.0						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON		_		0.66	1.04	0.67	0.00		
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6		
PARAPET JOINT DETAIL						 *	* *	F/S-10	S/S-15	S/S-15	F/S-10		
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVE	L CÜRB TO CURB	SKEW	ABUTMENT	PIER	PIER	ABUTMENT		
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2		
01772	1.5"	2"	95.33	I-95 NB OVER ROUTE 12,	1	30.0	36.0						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON		1		0.0	0.27	0.92	0.27		
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6		
PARAPET JOINT DETAIL							ماد ماد	F/S-10	S/S-15	S/S-15	F/S-10		
BRIDGE NO.	FINE MILLING	HMA SO.5	MILE	LOCATION	NO. OF TRAVE	L CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT				
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2				
01773	1.5"	2"	95.40	I-95 NB OVER SR900 (BONNIE CIRCLE),	3	50.0	7.0						
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.85	0.00				
DECK JOINT DETAIL								B/S-6	B/S-6				
PARAPET JOINT DETAIL								F/S-10	F/S-10				

QUANTITIES								
ITEM	UNITS	AMOUNTH						
REMOVAL OF HMA WEARING SURFACE	SY	2239						
CUT BITUMINOUS CONCRETE PAVEMENT	LF	7165						
TEMPORARY PAVEMENT	SY	2239						
HOT MIX ASPHALT S 0.375	TONS	406						
JOINT CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT	LF	7849						
MATERIAL FOR TACK COAT	GAL	243						
RECONSTRUCT CONCRETE DECK ENDS	CY	7						
BRIDGE EXPANSION JOINT ELASTOMERIC CONCRETE HEADERS	CF	81						
ASPHALTIC PLUG EXPANSION JOINT SYSTEM	CF	1485						
PREFORMED SILICONE JOINT SEALING SYSTEM	LF	184						
MEMBRANE WATERPROOFING (SHEET) (TORCH APPLIED)	SY	2178						

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-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED	
-	-	-	-	QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED	СН
-	-	-	-	INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE	
_	-	-	-	THE CONDITIONS OF ACTUAL QUANTITIES	
_	-	-	_	OF WORK WHICH WILL BE REQUIRED.	
_	-	-	-		
REV.	DATE	REVISION DESCRIPTION	SHEET NO). Plotted Date: 10/17/2013	

SIGNER/DRAFTER:
BU
ECKED BY:
RPL

SCALE AS NOTED



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	APPROVED BY:	
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PAVEMENT F	PRESERVATION
ON	I-95

TOWN:	PROJECT NO.
	58-329
TOWN OF GROTON	DRAWING NO. S-2
DRAWING TITLE:	3-2
BRIDGE INFORMATION	SHEET NO.
TABLE 1	04.02

[★] MEASURED PERPENDICULAR TO CENTERLINE ROADWAY. FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED TO THE TOTAL BRIDGE WIDTH.

^{**} MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.

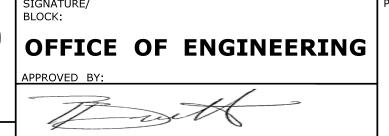
		BRID	GE INFO	RMATION FOR THE INSTALLAT	ON OF BRI	DGE DECK JO	INTS (TOW	N OF GROTO	N)									
	BRIDGE DATA							BRIDGE JOINT LOCATIONS										
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAV	* EL CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT			DICIDGE 5	01111 200					
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2									
01774	1.5"	2"	95.41	I-95 SB OVER SR900 (BONNIE CIRCLE),	3	50.0	31.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				1.02	0.00									
DECK JOINT DETAIL								B/S-6	B/S-6									
PARAPET JOINT DETAIL								F/S-10	F/S-10									
BRIDGE NO.	FINE MILLING	HMA SO.5	MILE	LOCATION	NO. OF TRAV	* EL CURB TO CURB	** SKEW	ABUTMENT	PIER	PIER	ABUTMENT							
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2							
01775	1.5"	1.5"	96.66	I-95 SB OVER BUDDINGTON ROAD,	3	51.8	16.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.29	0.59	0.29	0.00							
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6							
PARAPET JOINT DETAIL								F/S-10	S/S-15	S/S-15	F/S-10							
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAV	* EL CURB TO CURB	** SKEW	ABUTMENT	PIER	PIER	ABUTMENT							
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2							
01776	4 ["	1 ["	06.68	LOE NE OVER BURDINGTON BOAD	3	F1 0	16.0											
01776	1.5"	1.5"	96.68	I-95 NB OVER BUDDINGTON ROAD,	3	51.8	16.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.34	0.53	0.22	0.00							
DECK JOINT DETAIL					_			B/S-6	A/S-5	A/S-5	B/S-6							
PARAPET JOINT DETAIL						*	**	F/S-10	S/S-15	S/S-15	F/S-10		I			1		
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAV	EL CURB TO CURB		ABUTMENT	PIER	PIER	PIER	PIER	PIER	PIER	PIER	PIER	ABUTMENT	
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 3	NO. 4	NO. 5	NO. 6	NO. 7	NO. 8	NO. 1	
01777	1.5"	1.5"	96.73	I-95 NB OVER BUDDINGTON ROAD,	3	50.0	0.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0	
DECK JOINT DETAIL								A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	
															-			
PARAPET JOINT DETAIL						*	* *	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAV	EL CURB TO CURB	SKEW	ABUTMENT	PIER	PIER	PIER	PIER	PIER	ABUTMENT				
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 3	NO. 4	NO. 5	NO. 2				
01778	1.5"	1.5"	96.75	I-95 NB OVER GROTON RESERVOIR,	3	50.0	0.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.74	0.74	0.74	0.74	0.74	0.74	0.00				
DECK JOINT DETAIL								A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5				
PARAPET JOINT DETAIL								S/S-15	S/S-15	S/S-15		S/S-15	S/S-15					

★ MEASURED PERPENDICULAR TO CENTERLINE ROADWAY. FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED TO THE TOTAL BRIDGE WIDTH.

** MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.

					DESIGNER/DRAFTER:
_	-	-	-	THE INFORMATION, INCLUDING ESTIMATED	BU
_	_	-	-	QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED	
-	-	-	-	INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE	RPL
_	-	-	-	THE CONDITIONS OF ACTUAL QUANTITIES	
-	_	-	-	OF WORK WHICH WILL BE REQUIRED.	
_	-	-	-		SCALE AS NOTED
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013	





PAVEMENT PRESERVATION ON I-95

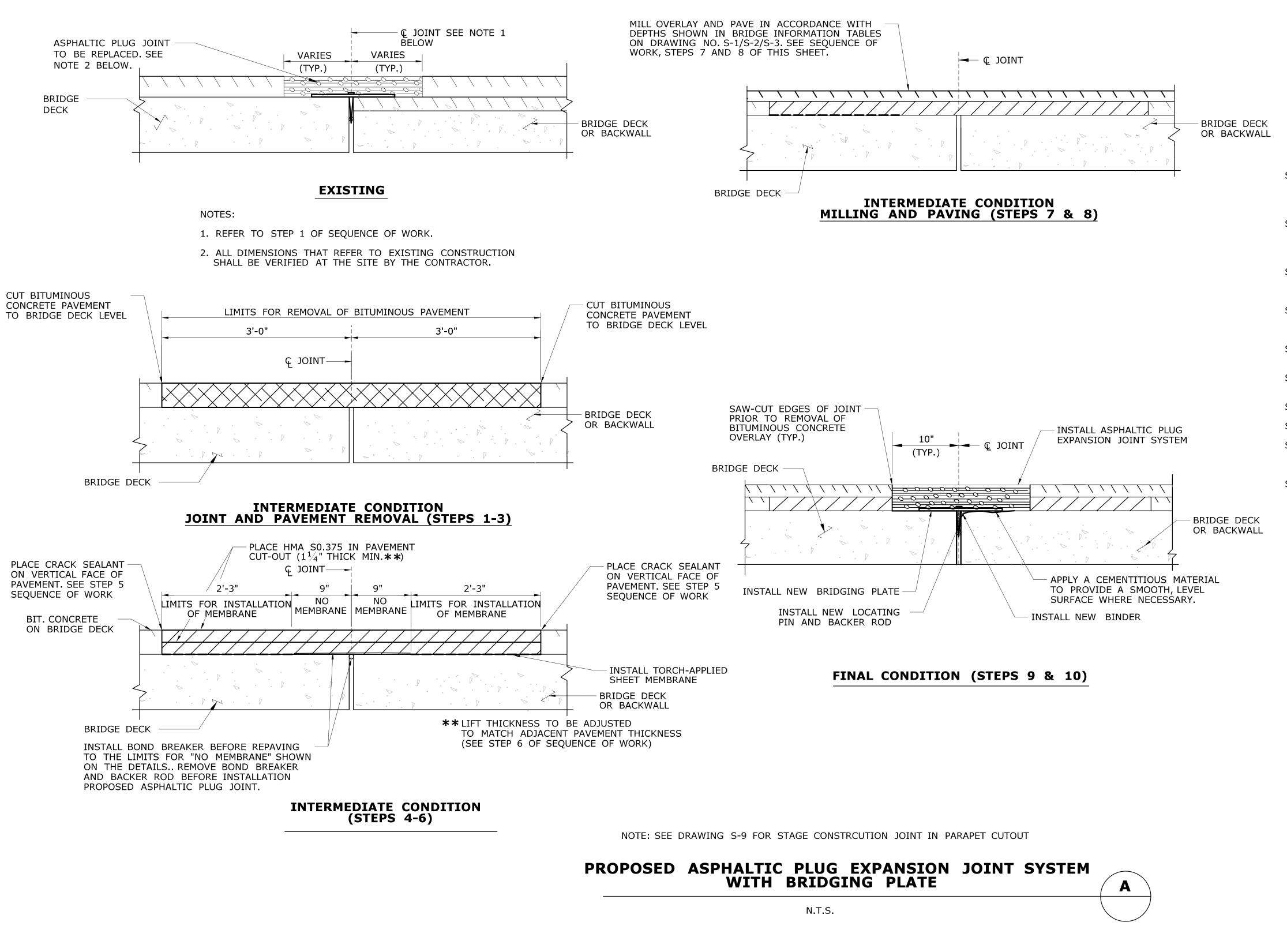
OWN:	PROJECT NO.
	58-329
TOWN OF GROTON	DRAWING NO. S-3
RAWING TITLE:	3-3
BRIDGE INFORMATION	SHEET NO.
TABLE 2	04.03

		BRIE	OGE INFOR	MATION FOR THE INSTALLAT	ION OF BRID	GE DECK JOIN	NTS (TOW	N OF GROTO	N)	
BRIDGE NO.				BRIDGE DATA				BRIDGE J	OINT LOC	ATIONS
					NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT	
	DEPTH (IN)	DEPTH (IN)	POINT	LOCATION	LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2	
01779	1.5"	2"	97.15	I-95 NB OVER POHEGNUT POND,	3	50.0	0.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.91	
DECK JOINT DETAIL								A/S-5	A/S-5	
PARAPET JOINT DETAIL								S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT	
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2	
01780	1.5"	2"	97.15	I-95 SB OVER POHEGNUT POND,	3	50.0	0.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.95	
DECK JOINT DETAIL								A/S-5	A/S-5	
PARAPET JOINT DETAIL								S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	PIER	ABUTMENT
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2
01781	1.5"	2"	97.48	I-95 NB ROUTE 117,	3	50.0	8.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.67	0.68
DECK JOINT DETAIL								B/S-6	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	F/S-10
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	PIER	ABUTMENT
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2
01782	1.5"	2"	97.49	I-95 SB ROUTE 117,	4	60.0	9.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.68	0.68
DECK JOINT DETAIL								B/S-6	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	F/S-10

[★] MEASURED PERPENDICULAR TO CENTERLINE ROADWAY. FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED TO THE TOTAL BRIDGE WIDTH.

		DESIGNER/DRAFTER:	NECT.	SIGNATURE/	PROJECT TITLE:	TOWN:	PROJECT NO.
	- THE INFORMATION, INCLUDING ESTIN	ATED BU		BLOCK:			58-329
	QUANTITIES OF WORK, SHOWN ON SHEETS IS BASED ON LIMITED	HESE CHECKED BY:	STATE OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	- INVESTIGATIONS BY THE STATE AND IN NO WAY WARRANTED TO INDICA	IS RPL	SIAIE OF CONNECTICOT	OTTICE OF ENGLISHER	PAVEMENT PRESERVATION		S-4
- - -	THE CONDITIONS OF ACTUAL QUANT	TIES	DEPARTMENT OF TRANSPORTATION	APPROVED BY:	_ PAVENTENT PRESERVATION	DRAWING TITLE:	•
	OF WORK WHICH WILL BE REQUIRED	·	DEPARTMENT OF TRANSPORTATION		ON I-95	BRIDGE INFORMATION	SHEET NO.
	-	SCALE AS NOTED		11 well	ON 1-95	TABLE 3	04.04
REV. DATE REVISION DESC	RIPTION SHEET NO. Plotted Date: 10/17/2013		Filename:\SB_MSH_S-4_BRIDGE_INFORMATION_TABLE_3.dgn			IADLL 3	1 04.04

^{**} MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.

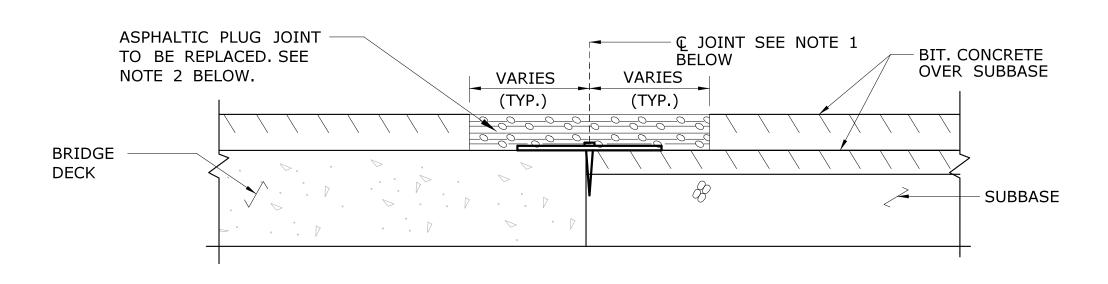


SUGGESTED SEQUENCE OF WORK

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTER LINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.
- STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
- STEP 3: REMOVE EXISTING PAVEMENT MATERIAL, JOINT MATERIAL (BINDER, BRIDGING PLATE, LOCATING PIN AND BACKER ROD) WITHIN THE LIMITS
- STEP 4: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN. INSTALL BOND BREAKER BEFORE REPAVING TO THE LIMITS FOR "NO MEMBRANE" SHOWN ON THE DETAILS.
- STEP 5: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
- STEP 6: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
- STEP 7: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.
- STEP 8: PAVE TOP COURSE ON ROADWAY AND BRIDGE.
- STEP 9: CUT PAVEMENT FULL DEPTH, 10" EACH SIDE OF CENTER OF JOINT, AND REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND THE BOND
- STEP 10: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM.



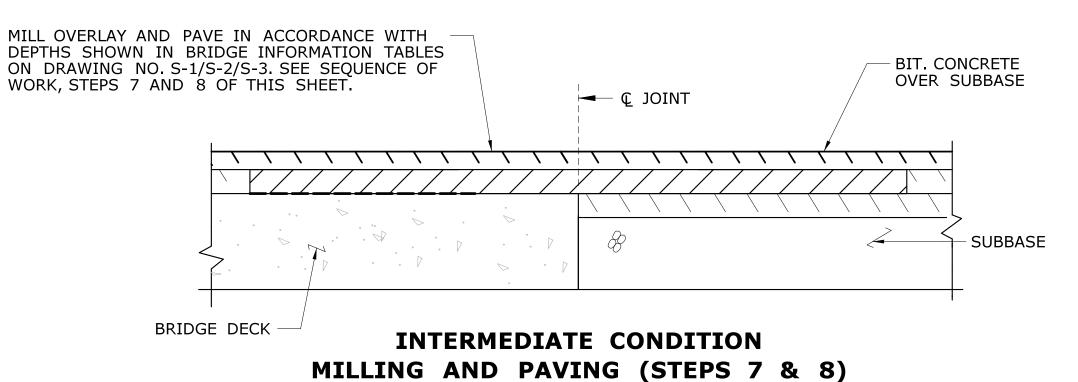
		DESIGNER/DRAFTER:		NNECTION	SIGNATURE/	PROJECT TITLE:	TOWN:	PROJECT NO.
	-	THE INFORMATION, INCLUDING ESTIMATED BU			BLOCK:			58-329
	_	QUANTITIES OF WORK, SHOWN ON THESE CHECKED BY:	CTATE C	OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	-	INVESTIGATIONS BY THE STATE AND IS IN NO. WAY WARPANTED TO INDICATE RPL	SIAIE C	OF CONNECTICOT	OTTICE OF ENGINEERING	PAVEMENT PRESERVATION	TOWN OF GROTON	
	_	THE CONDITIONS OF ACTUAL QUANTITIES	DEDARTMENT	OF TRANSPORTATION	APPROVED BY:	PAVEMENT PRESERVATION	DRAWING TITLE:	5- 5
	_	OF WORK WHICH WILL BE REQUIRED.		OI INANSPORTATION	=======================================	ON I-95	ASPHALTIC PLUG	SHEET NO.
	_	SCALE AS I	NOTED		1 well	ON 1-95	EXPANSION JT. DETAILS 1	1 04.05
REV. DATE REVISION DESCRIPTION	SHEET NO	Plotted Date: 10/17/2013	Filename:\SB_MSH_S-5_ASPHA	ALTIC_PLUG_EXPANSAION_JOINT_DETAIL_1.dgn			EXPANSION 11. DETAILS 1	L 04.03

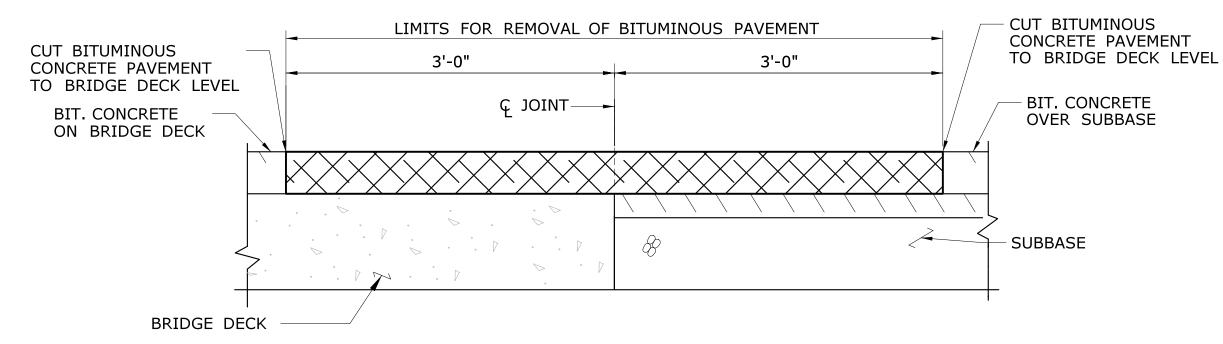


EXISTING

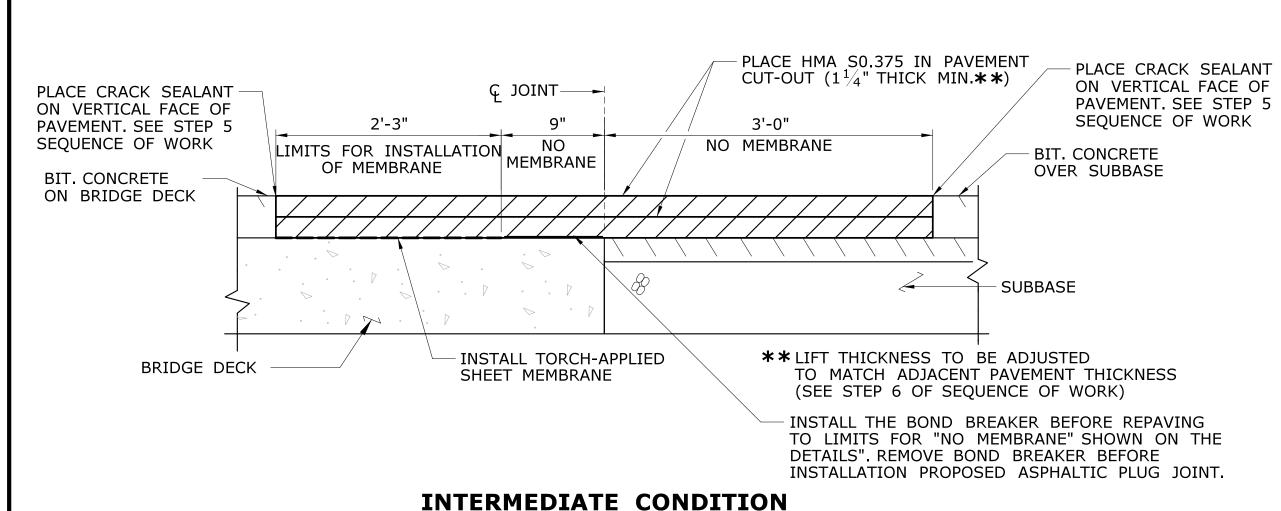
NOTES:

- 1. REFER TO STEP 1 OF SEQUENCE OF WORK.
- 2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED AT THE SITE BY THE CONTRACTOR.

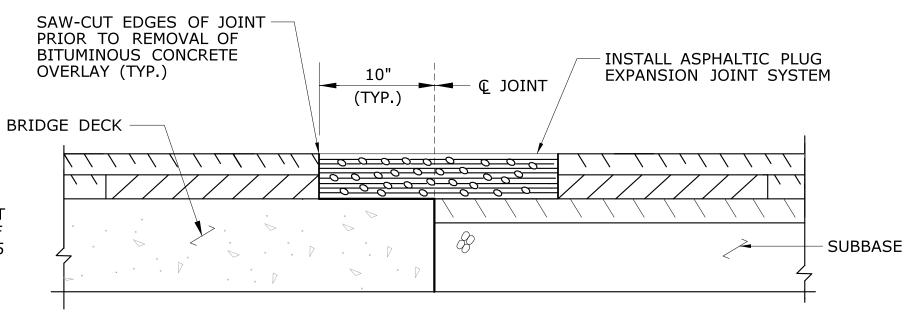




INTERMEDIATE CONDITION JOINT AND PAVEMENT REMOVAL (STEPS 1-3)



(STEPS 4-6)



FINAL CONDITION (STEPS 9 & 10)

SUGGESTED SEQUENCE OF WORK

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.
- STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
- STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL (BINDER, BRIDGING PLATE, LOCATING PIN AND BACKER ROD) WITHIN THE LIMITS SHOWN.
- STEP 4: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN.
 INSTALL THE BOND BREAKER BEFORE REPAVING TO LIMITS FOR "NO
 MEMBRANE" SHOWN ON THE DETAILS".
- STEP 5: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
- STEP 6: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
- STEP 7: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.
- STEP 8: PAVE TOP COURSE ON ROADWAY AND BRIDGE.
- STEP 9: CUT PAVEMENT FULL DEPTH, 10" EACH SIDE OF CENTER OF JOINT, AND REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND THE BOND BREAKER.
- STEP 10: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM.

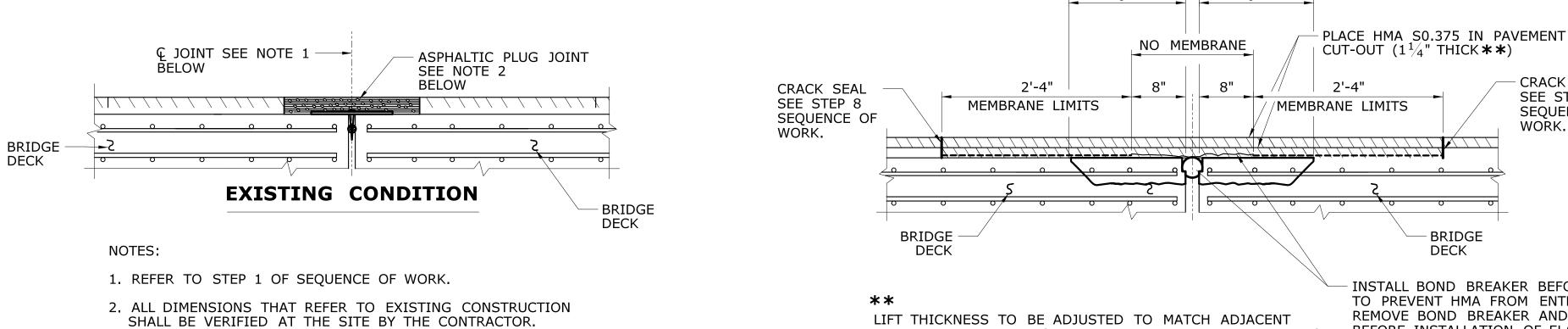
NOTE: SEE DRAWING S-9 FOR STAGE CONSTRCUTION JOINT IN PARAPET CUTOUT

PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM WITHOUT BRIDGING PLATE

N.T.S.



STATE OF CONNECTICUT 58-329 THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED OFFICE OF ENGINEERING - - -TOWN OF GROTON INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE - | - | -**PAVEMENT PRESERVATION S-6** _ | - | -THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. **DEPARTMENT OF TRANSPORTATION ASPHALTIC PLUG** - - -ON I-95 SCALE AS NOTED **EXPANSION JT. DETAILS 2** 04.06 REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/17/2013 Filename: ...\SB_MSH_S-6_ASPHALTIC_PLUG_EXPANSAION_JOINT_DETAIL_2.dgn



- CUT BITUMINOUS CONCRETE

LEVEL

BRIDGE

BRIDGE

1" DEEP VERTICAL

REMOVE DETERIORATED CONCRETE AND RECONSTRUCT THE DECK ENDS

WITH A STEP FOR THE JOINT SEAL AS SHOWN IN DETAIL 'A'. THIS WORK TO BE PAID FOR UNDER THE ITEM,

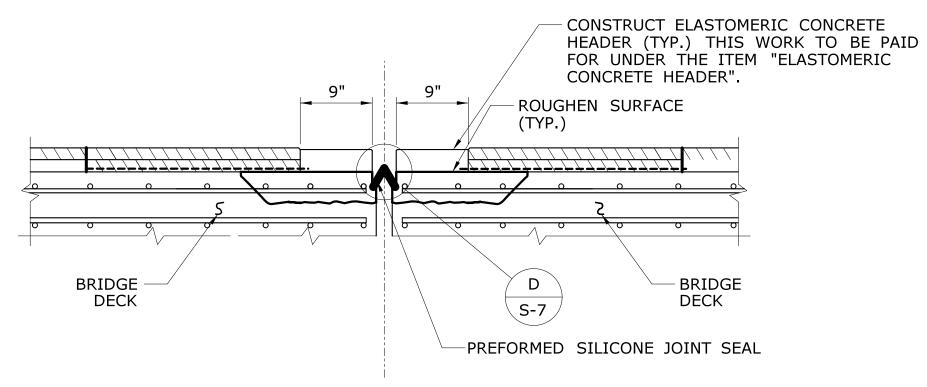
"RECONSTRUCT CONCRETE DECK ENDS"

SAW-CUT THROUGHOUT

DECK

PAVEMENT TO BRIDGE DECK

INTERMEDIATE CONDITION (STEPS 6-9)



STEP 14: APPLY CRACK SEAL TO PAVEMENT SURFACE AT SAW-CUT JOINTS AND AT THE CURB LINE. SILICONE BEAD (TYP.) INSTALLATION DEPTH PER MANUFACTURER'S **SPECIFICATIONS** (TYP.)

NOTE: SEE DRAWING S-9 FOR STAGE CONSTRCUTION JOINT IN PARAPET CUTOUT

PROPOSED PREFORMED SILICONE JOINT SEAL AT PIER NOS 1 AND 2, BRIDGE NO. 01770 N.T.S.

(STEPS 4-5)

SHEET NO. Plotted Date: 10/17/2013

INTERMEDIATE CONDITION

LIMITS REMOVAL OF BITUMINOUS PAVEMENT

INTERMEDIATE CONDISTION

JOINT AND PAVEMENT REMOVAL (STEPS 1-3)

LIMITS FOR REMOVAL OF BITUMINOUS PAVEMENT AND JOINT MATERIAL

EQUAL

Ç JOINT →

Ç JOINT ──►

3'-0"

3'-0"

CUT BITUMINOUS

TO BRIDGE DECK

LEVEL

CONCRETE PAVEMENT

BRIDGE

1" REMOVE CONCRETE BELOW

REINFORCING, TYP. (MIN.)

REV. DATE

DECK

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED - - -INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE - | - | -_ | - | -THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. - - - -

REVISION DESCRIPTION

DECK

SCALE AS NOTED Filename: ...\SB_MSH_S-7_PREFORMED_SILICONE_JOINT_DETAIL.dgn

STATE OF CONNECTICUT **DEPARTMENT OF TRANSPORTATION**

OFFICE OF ENGINEERING

TOWN OF GROTON PREFORMED SILICONE JOINT SEAL DETAILS

SHEET NO. 04.07

58-329

S-7

PAVEMENT PRESERVATION ON I-95

MEMBRANE LIMITS BRIDGE DECK INSTALL BOND BREAKER BEFORE REPAVING TO PREVENT HMA FROM ENTERING JOINT, REMOVE BOND BREAKER AND BACKER ROD BEFORE INSTALLATION OF ELASTOMERIC PAVEMENT THICKNESS (SEE STEP 9 OF SEQUENCE OF WORK) CONCRETE HEADER

STEP 7: INSTALL BOND BREAKER AT THE LOCATION OF THE

PROPOSED ELASTOMERIC CONCRETE HEADER. STEP 8: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVE-MENT ALONG SAW-CUT LINES.

SUGGESTED SEQUENCE OF WORK

AT THE GUTTERLINE TO DETERMINE THE DEPTH

OF PAVEMENT AND THE LOCATION OF THE DECK

END (CENTERLINE OF PROPOSED JOINT) BEFORE

OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE END OF THE

FOR 1" BELOW THE REINFORCEMENT AS SHOWN IN

DECK. CREATE A STEP AS SHOWN IN DETAIL S-

INSTALL MEMBRANE TO THE TOP OF DECK WITHIN

DECKS. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.

MATERIALS (BINDER, BRIDGING PLATE, LOCATING PIN AND

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT

BACKER ROD) WITHIN THE LIMITS SHOWN.

STEP 4: REMOVE SUFFICIENT CONCRETE MATERIAL TO ALLOW

STEP 5: POUR CONCRETE TO THE ELEVATION OF THE BRIDGE

PROCÈEDING TO STEP 2.

THE DETAIL.

THE LIMITS SHOWN.

STEP 9: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)

STEP 10: CUT PAVEMENT FULL DEPTH, 9" FROM THE BRIDGE DECK ENDS AS SHOWN IN THE DETAIL REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND THEN REMOVE THE BOND BREAKER.

STEP 11: CLEAN AND RE-SEAL THE JOINT BETWEEN THE DECK ENDS.

STEP 12: INSTALL THE PROPOSED ELASTOMERIC HEADER AS SHOWN IN THE DETAIL.

STEP 13: INSTALL THE SILICOFLEX JOINT SEAL.

FINAL CONDITION (STEP 10-14)

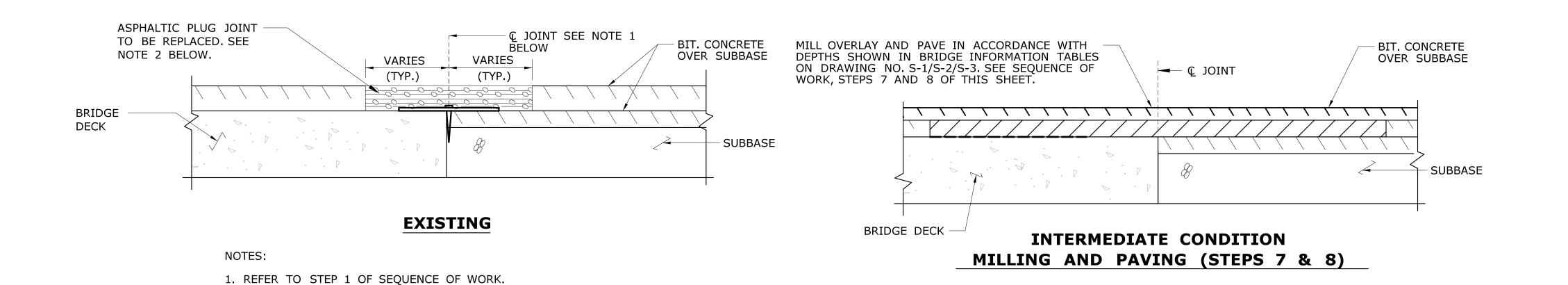
CRACK SEAL

SEE STEP 8

WORK.

SEQUENCE OF

PREFORMED SILICONE JOINT SEAL **S-7**

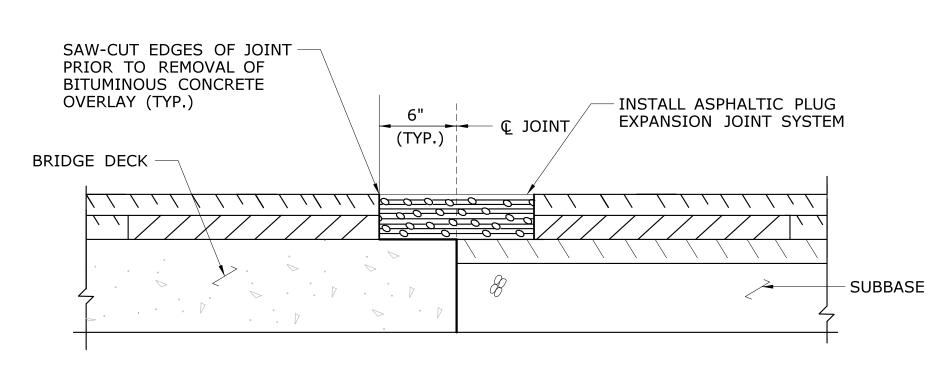


CUT BITUMINOUS CONCRETE PAVEMENT TO BRIDGE DECK LEVEL BIT. CONCRETE ON BRIDGE DECK CUT BITUMINOUS CONCRETE PAVEMENT TO BRIDGE DECK LEVEL BIT. CONCRETE OVER SUBBASE SUBBASE

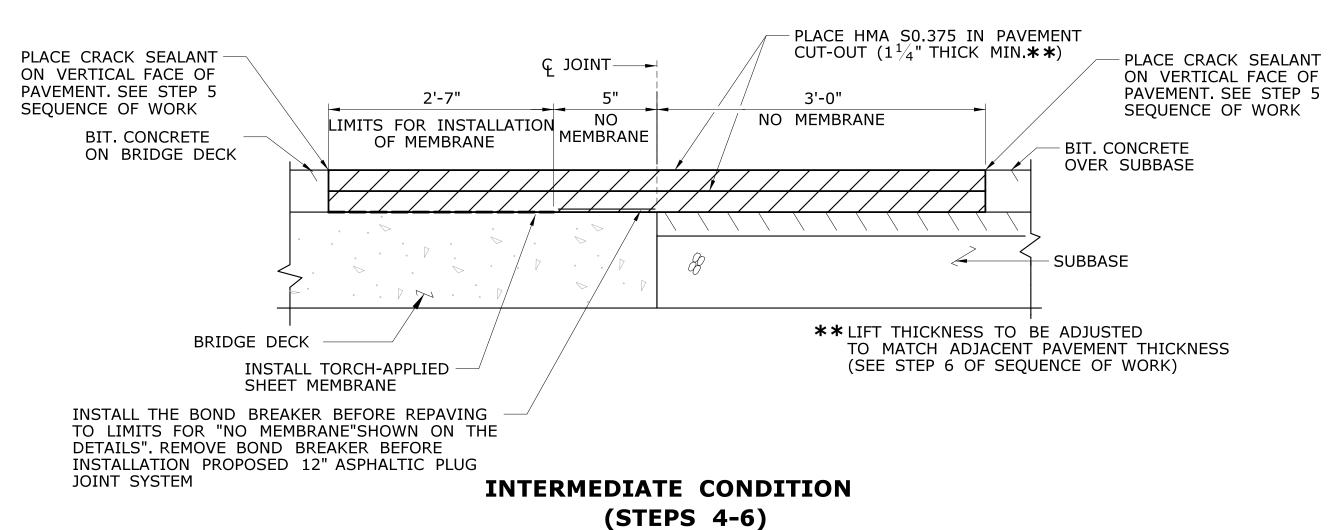
2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED AT THE SITE BY THE CONTRACTOR.

INTERMEDIATE CONDITION

JOINT AND PAVEMENT REMOVAL (STEPS 1-3)



FINAL CONDITION (STEPS 9 & 10)



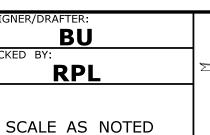
NOTE: SEE DRAWING S-9 FOR STAGE CONSTRCUTION JOINT IN PARAPET CUTOUT

PROPOSED 12" ASPHALTIC PLUG EXPANSAION JOINT SYSTEM AT ABUTMENT NOS 1 AND 2, BRIDGE NO. 01770

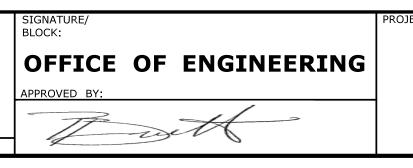
N.T.S.

						DES:
-	-	-	-		THE INFORMATION, INCLUDING ESTIMATED	
_	-	-	_		QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED	CHE
-	-	-	-		INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE	
-	-	-	-		THE CONDITIONS OF ACTUAL QUANTITIES	
-	-	-	-		OF WORK WHICH WILL BE REQUIRED.	
ı	-	-	-			
REV.	DATE	REVISION DESCRIPTION	SHEET	NO.	Plotted Date: 10/17/2013	1

BRIDGE DECK







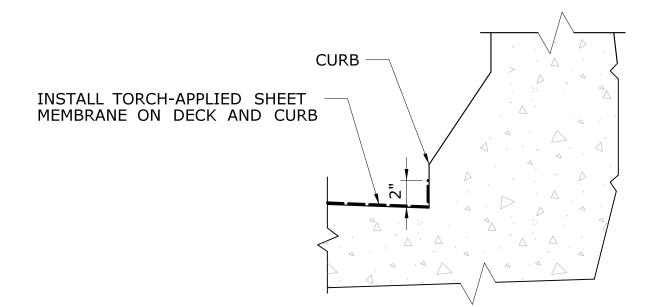
Ε

PAVEMENT PRESERVATION ON I-95

TOWN:	PROJECT NO.
	58-329
TOWN OF GROTON	DRAWING NO.
DRAWING TITLE:	7 3-6
ASPHALTIC PLUG	SHEET NO.
EXPANSION JT. DETAILS	04.08

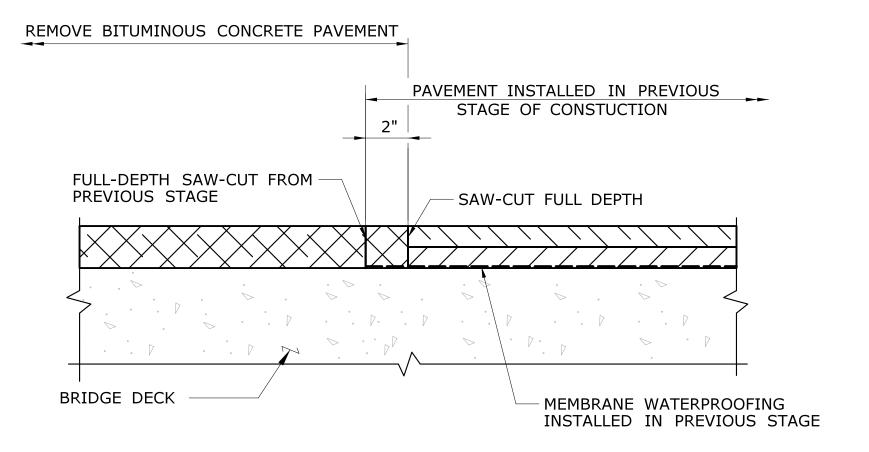
SUGGESTED SEQUENCE OF WORK

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTER LINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.
- STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
- STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL (BINDER, BRIDGING PLATE, LOCATING PIN AND BACKER ROD) WITHIN THE LIMITS SHOWN.
- STEP 4: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN. INSTALL THE BOND BREAKER BEFORE REPAVING TO LIMITS FOR "NO MEMBRANE" SHOWN ON THE DETAIL.
- STEP 5: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
- STEP 6: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
- STEP 7: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.
- STEP 8: PAVE TOP COURSE ON ROADWAY AND BRIDGE.
- STEP 9: CUT PAVEMENT FULL DEPTH, 6" EACH SIDE OF CENTER OF JOINT REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND BOND BREAKER.
- STEP 10: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM.



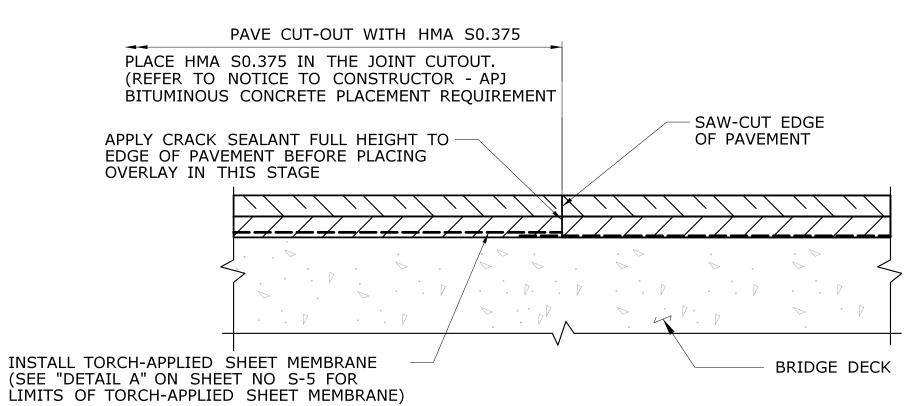
SECTION AT GUTTERLINE AT PAVEMENT CUT OUT

N.T.S.



SECTION - INITIAL LONGITUDINAL STAGE CONSTRUCTION JOINT IN PAVEMENT CUTOUT

N.T.S.



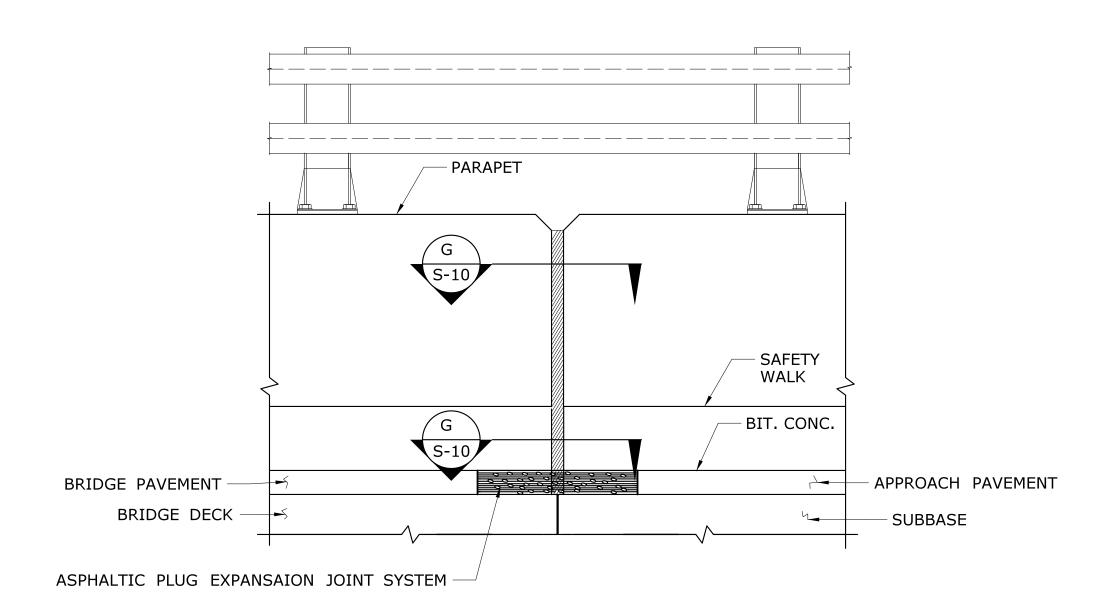
SECTION - FINAL LONGITUDINAL STAGE CONSTRUCTION JOINT IN PAVEMENT CUTOUT

N.T.S.

STAGING CONSTRUCTION JOINT IN PAVEMENT CUTOUT

N.T.S.

		DESIGNER/DR	RAFTER:	A SPORT A	SIGNATURE/	PROJECT TITLE:	TOWN:	PROJECT NO.
	-	THE INFORMATION, INCLUDING ESTIMATED	BU		BLOCK:			58-329
	-	SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS	RPL =	STATE OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	-	IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES		OF TRANS	APPROVED BY:	PAVEMENT PRESERVATION	DRAWING TITLE:	⊣ S-9
	-	OF WORK WHICH WILL BE REQUIRED.		DEPARTMENT OF TRANSPORTATION		ON TOE	STAGE CONSTRUCTION	SHEET NO.
	-	SCALI	E AS NOTED		I well	ON I-95	JT. IN PAVEMENT CUTOUT	04.09
REV. DATE	REVISION DESCRIPTION SHEET NO.	Plotted Date: 10/17/2013	File	llename:\SB_MSH_S-9_STAGE_CONSTRUCTION_JOINT_PAVEMENT_CUTOUT_DETAIL.dgn			DITTA PASTIFICAL COLOGI	1 04.09

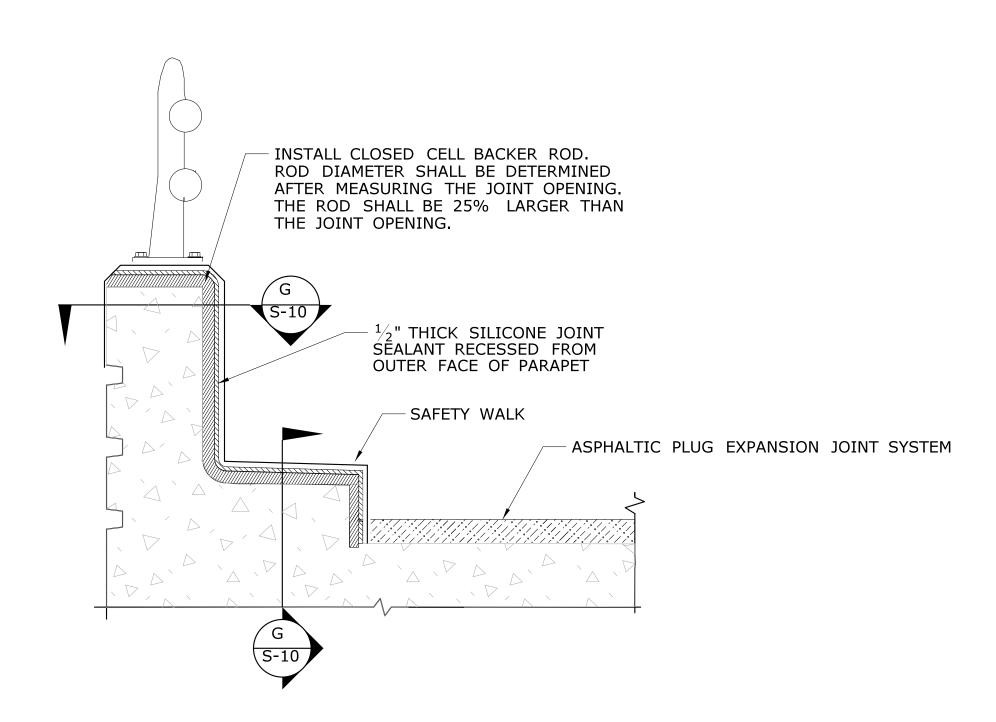


JOINT TREATMENT AT PARAPET

NOT TO SCALE

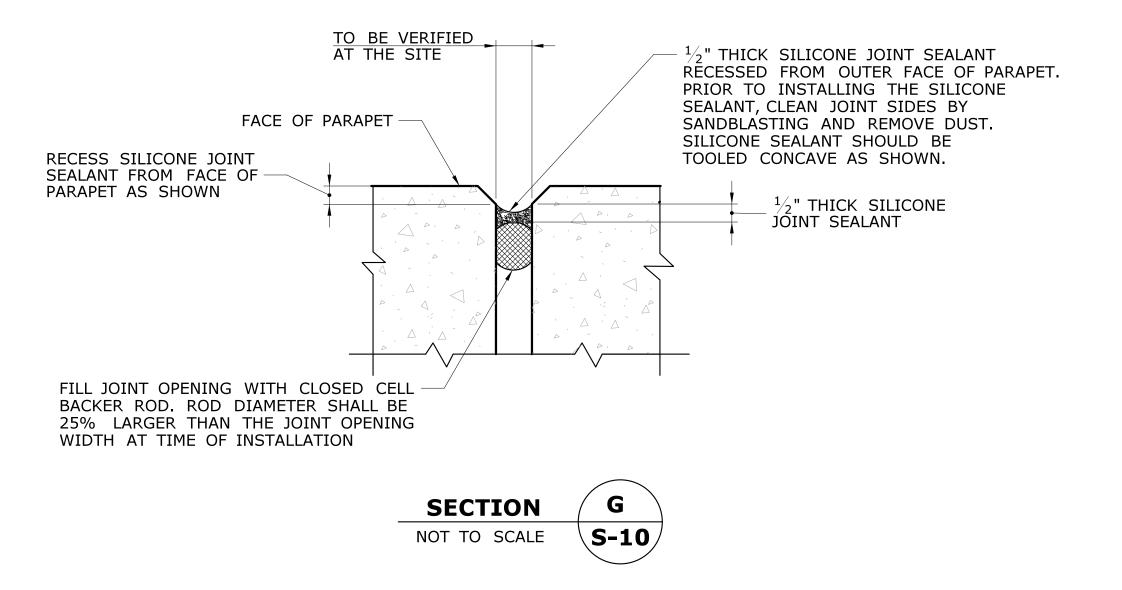
NOTE:

PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".



JOINT TREATMENT AT PARAPET

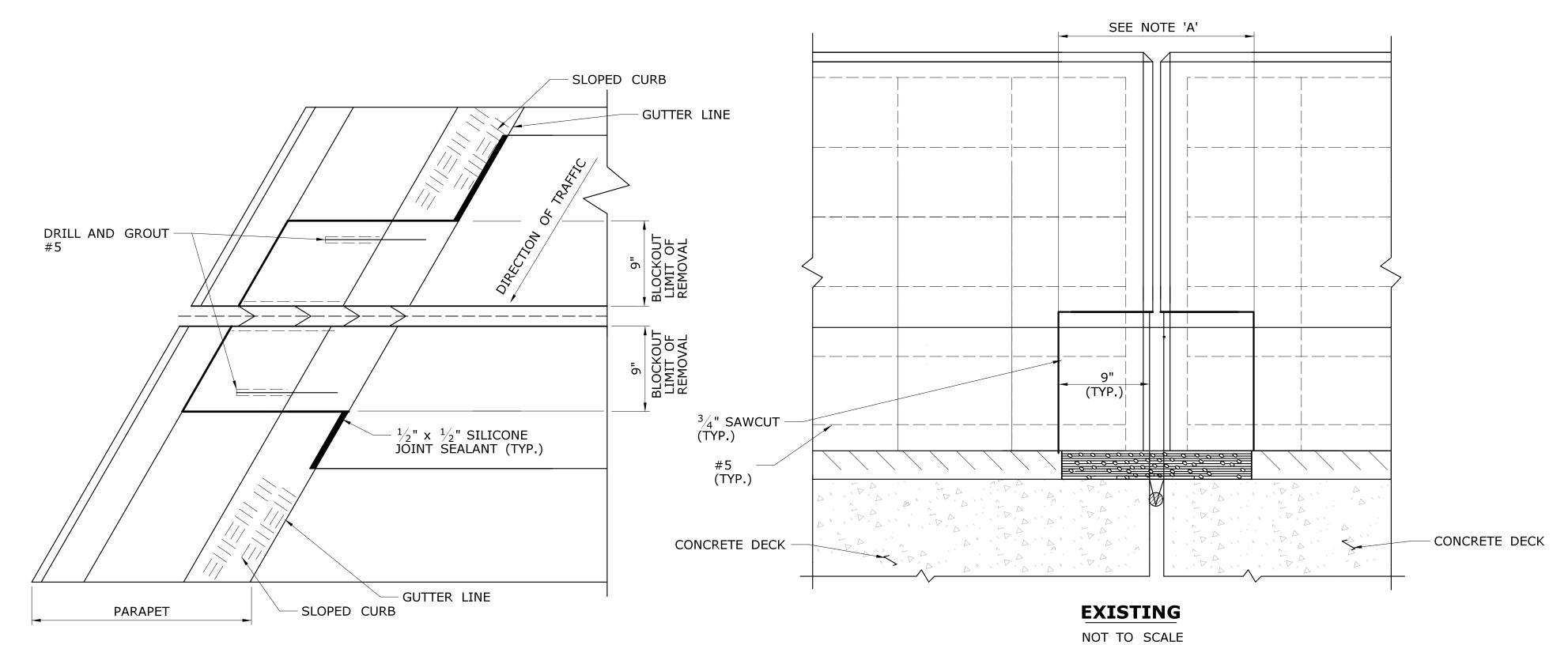
NOT TO SCALE





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	THE INFORMATION, INCLUDING ESTIMATED	BU		BLOCK:			58-329
	SHEETS IS BASED ON LIMITED	CHECKED BY:	STATE OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	- INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE	RPL	STAIL OF CONNECTION		PAVEMENT PRESERVATION	TOTAL OF GROTOIT	─ S-10
	THE CONDITIONS OF ACTUAL QUANTITIES		DEPARTMENT OF TRANSPORTATION	APPROVED BY:	PAVEMENT PRESERVATION	DRAWING TITLE:	3-10
	OF WORK WHICH WILL BE REQUIRED.		DEPARTMENT OF TRANSPORTATION		ON I-95	PARAPET JOINT	SHEET NO.
	-	SCALE AS NOTED		11 well	ON 1-95		04.010
REV. DATE REVISION DESCRIPTION	SHFFT NO. Plotted Date: 10/17/2013	7	Filename:\SB_MSH_S-10_PARAPEt_JOINT_DETAIL_1.dgn			DETAILS 1	04.010

REMOVE ASPHALTIC PLUG EXPANSION JOINT SYSTEM, AND CONCRETE WITHIN THESE PARAPET LIMITS. THIS WORK TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "RECONSTRUCT DECK ENDS". SEE LIMITS OF REMOVAL IN PARTIAL PLAN ON THIS DRAWINGS.



PARTIAL PLAN - JOINT TREATMENT IN PARAPET

NOT TO SCALE

PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING, DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".

SUGGESTED SEQUENCE OF WORK

STEP 1: $\frac{3}{4}$ " SAW-CUT SAFETY CURB AND PARAPET AS SHOWN IN THE DETAILS.

STEP 2: REMOVE ANY ADDITIONAL CONCRETE NECESSARY TO OBTAIN THE REQUIRED REINFORCEMENT CLEARANCES AND CONCRETE COVER SHOWN IN THE DETAILS.

STEP 3: DRILL AND GROUT ANGLE DOWELS INTO THE BRIDGE DECK AS SHOWN IN THE DETAILS. PLACE TRANSVERSE REINFORCEMENT ON GROUTED DOWELS AS SHOWN IN THE DETAILS. SEE SECTION 'D' ON DRAWING S-12

STEP 4: PLACE CONCRETE FOR THE PROPOSED CONCRETE SAFETY CURB AND PARAPET. CONSTRUCT THE JOINT IN THE SAFETY CURB AND PARAPET AS SHOWN IN THE DETAIL.

PROPOSED

NOT TO SCALE

9" TYP.

RECONSTRUCT PARAPET FOR JOINT SEAL. THIS WORK PAID FOR UNDER THE ITEM "RECONSTRUCT CONCRETE

- CONCRETE DECK

DECK ENDS".

* IF CONCRETE COVER IS LESS THAN 2", REMOVE EXISTING REINFORCEMENT, THEN DRILL AND GROUT DOWELS FOR NEW REINFORCEMENT.

JOINT TREATMENT IN PARAPET AT PIER NOS. 1 AND 2 ,BR. NO. 01770

NOT TO SCALE

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. - - -- | - | -_ | _ _ _ - - -REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/17/2013

BU RPL SCALE AS NOTED

STATE OF CONNECTICUT **DEPARTMENT OF TRANSPORTATION** Filename: ...\SB_MSH_S-11_PARAPET_JOINT_DETAIL_2A.dan

SIGNATURE/ BLOCK: OFFICE OF ENGINEERING

TOWN OF GROTON PARAPET JOINT

PAVEMENT PRESERVATION ON I-95

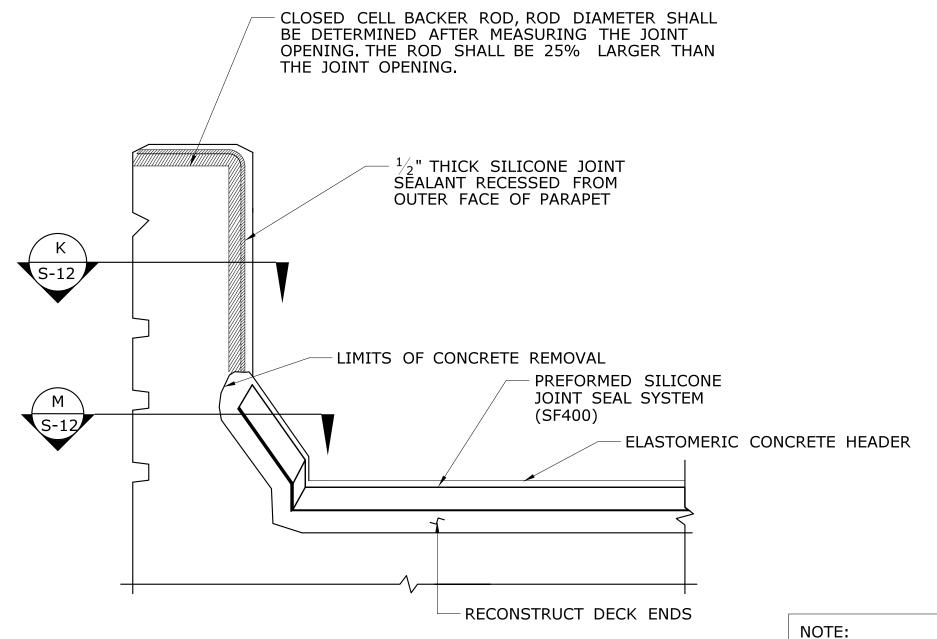
58-329 S-11 SHEET NO. **DETAILS 2A** 04.011

#5 **EACH** SIDE OF JOINT

- CONCRETE DECK

2"MIN.*

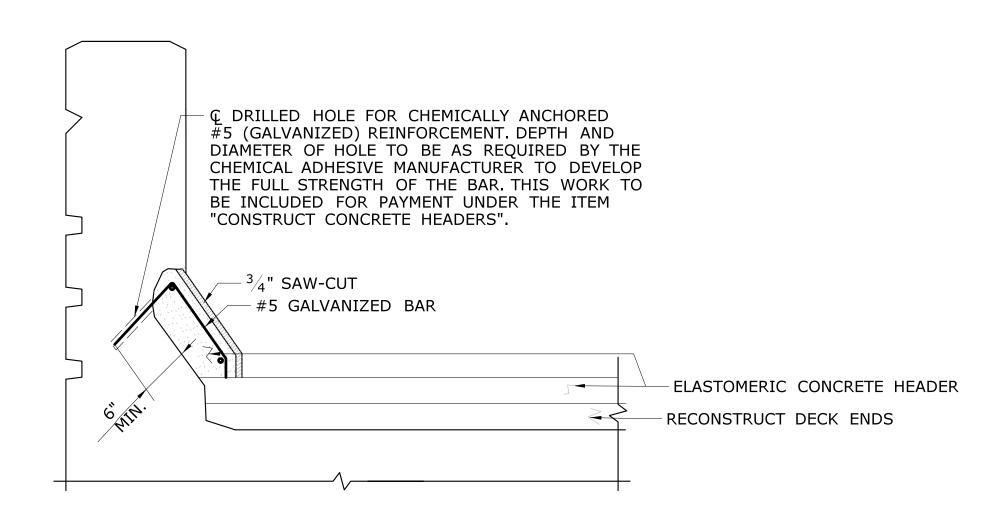
#5 (TYP.)

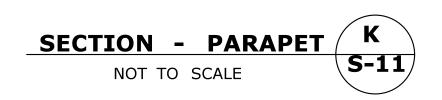


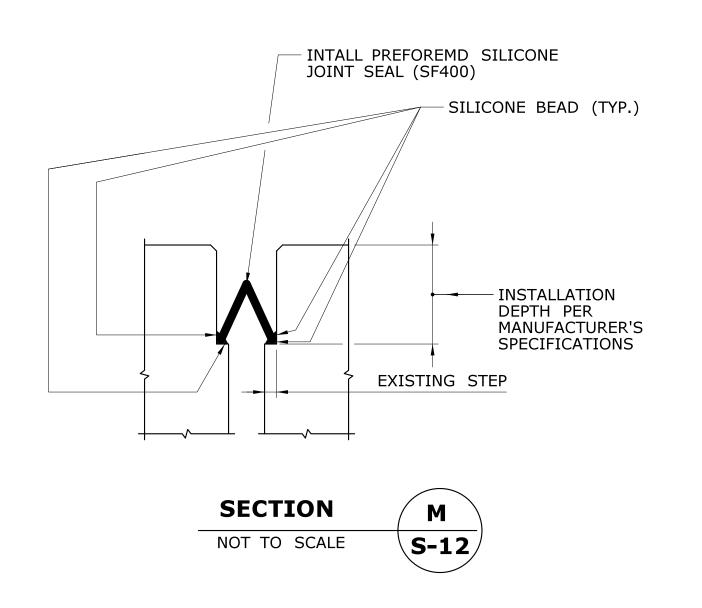
SECTION - PARAPET J

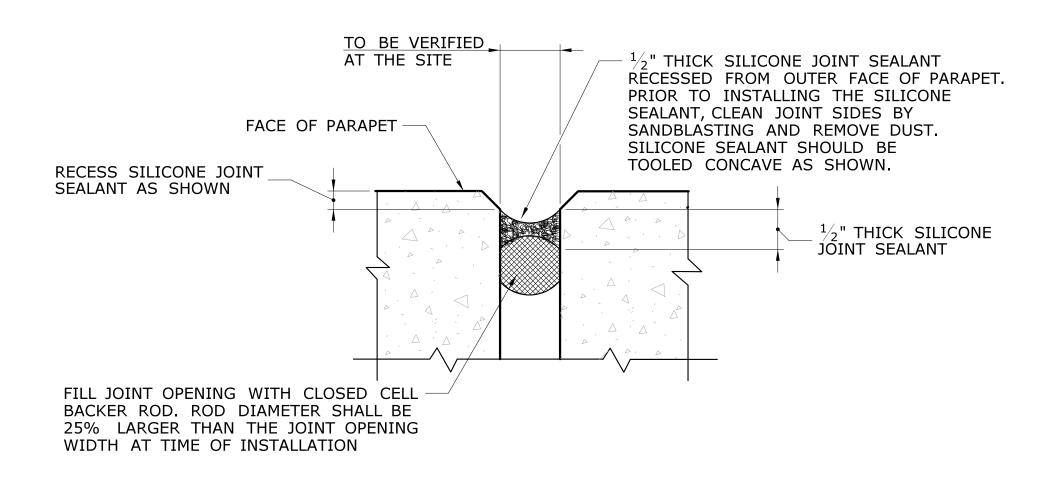
NOT TO SCALE S-11

PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".







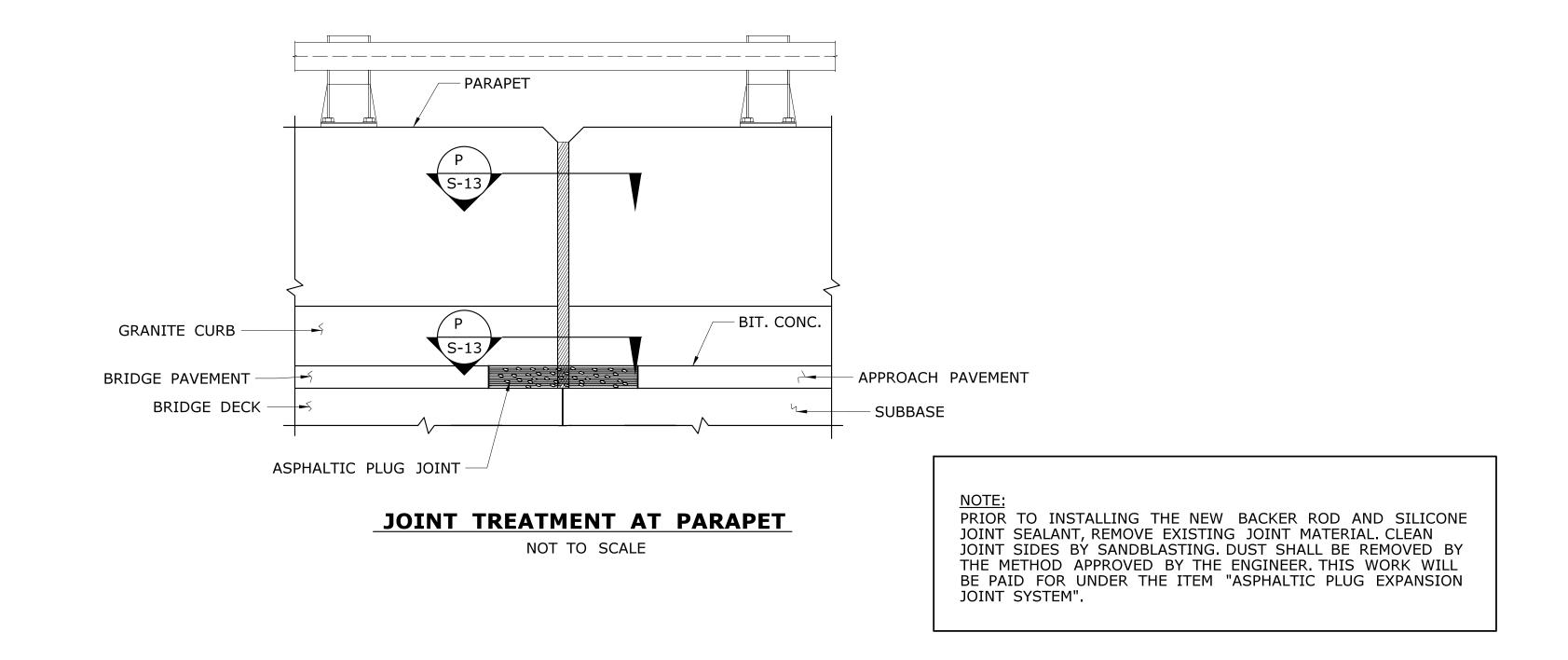


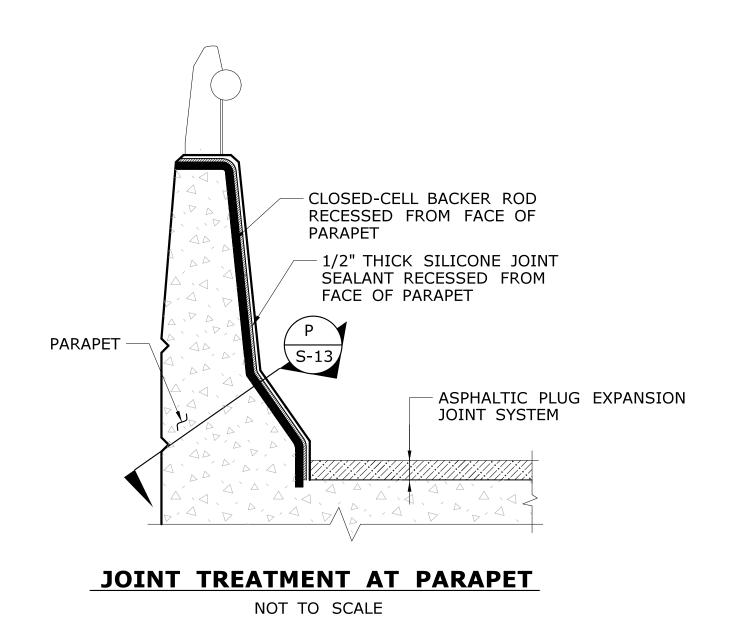
SECTION THROUGH PARAPET OR MEDIAN BARRIER JOINT N
LARGE OPENING
NOT TO SCALE
S-11

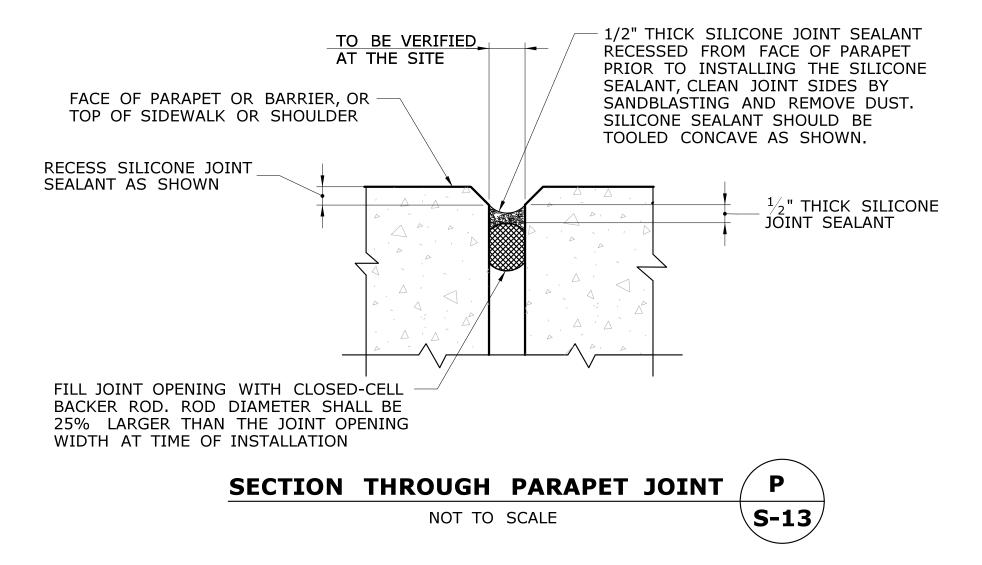
JOINT TREATMENT IN PARAPET AT PIER NOS. 1 AND 2 ,BR. NO. 01770

NOT TO SCALE

		DESIGNER/DRAFTER:		I CICNATURE/	I DDOJECT TITLE.	LTOWN	I DD O I E CT NO
_	THE INCORMATION INCLUDING ESTIMAT	RII	CONNECTICU	BLOCK:	PROJECT TITLE.	TOWN.	FO 23
	QUARTE OF WORK, SHOWN ON THE	CHECKED BY:		OFFICE OF ENGINEERING		TOWN OF CROTON	58-32
	- INVESTIGATIONS BY THE STATE AND IS	RPL	STATE OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	THE CONDITIONS OF ACTUAL QUANTITI	S	DEPARTMENT OF TRANSPORTATION	APPROVED BY:	PAVEMENT PRESERVATION	DRAWING TITLE:	─ S-1
	OF WORK WHICH WILL BE REQUIRED.		DEPARTMENT OF TRANSPORTATION	=======================================	ON I-95	PARAPET JOINT	SHEET NO.
	-	SCALE AS NOTED		11-well	ON 1-95	DETAILS 2B	04.012
V DATE REVISION DESCRIPTION	SHEET NO I Plotted Date: 10/17/2013		Filename:\SB_MSH_S-12_PARAPET_JOINT_DETAIL_2B.dan			DEIAILS 2D	1 04.01



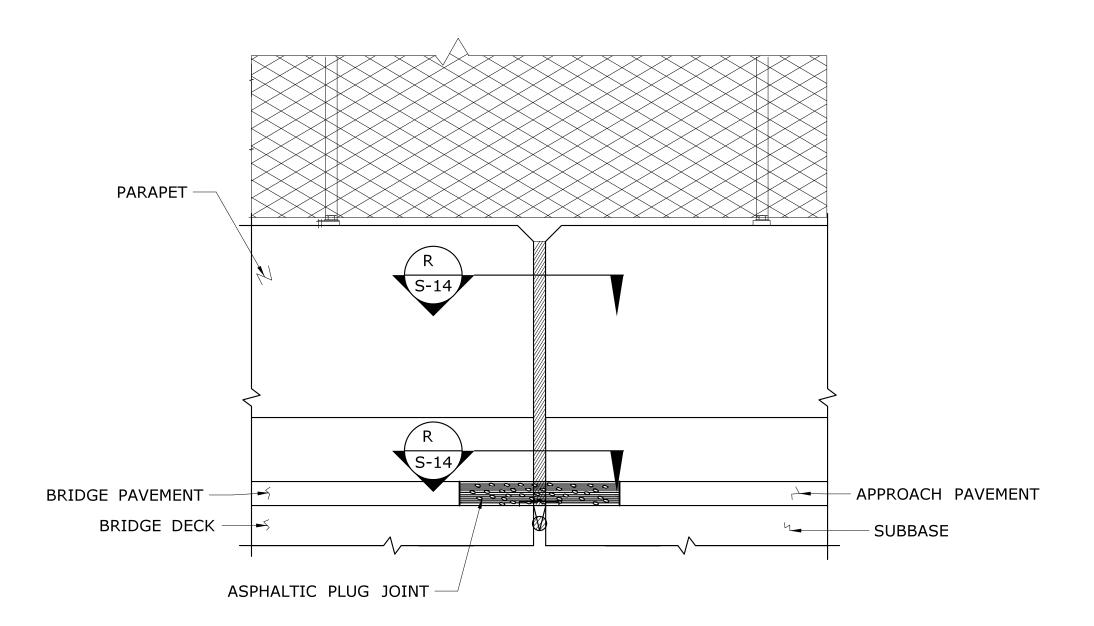




SILICONE JOINT SEALANT AND BACKER ROD DETAILS AT PARAPET AND MEDIAN BARRIER, BR. NO. 02445

NOT TO SCALE

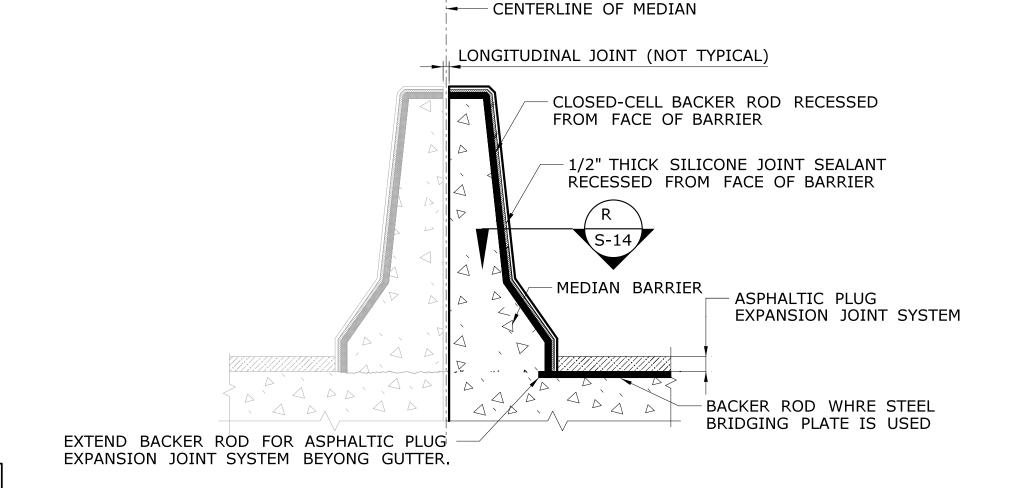
		DESIGNER/DRAFTER:		SIGNATURE/	PROJECT TITLE:	TOWN:	PROJECT NO.
	THE INFORMATION, INCLUDING ESTIMA	ED BU		BLOCK:			58-329
	SHEETS IS BASED ON LIMITED	CHECKED BY:	STATE OF CONNECTICUT	OFFICE OF ENGINEERING		TOWN OF GROTON	DRAWING NO.
	INVESTIGATIONS BY THE STATE AND I	KPL KPL	JIAIL OI CONNECTICO		PAVEMENT PRESERVATION		─ S-13
	- THE CONDITIONS OF ACTUAL QUANTITY	ES	DEPARTMENT OF TRANSPORTATION	APPROVED BY:	PAVEMENT PRESERVATION	DRAWING TITLE:	<u> </u>
	_ OF WORK WHICH WILL BE REQUIRED.		DEFARIMENT OF TRANSPORTATION		ON I-95	PARAPET JOINT	SHEET NO.
	-	SCALE AS NOTED		1 Just	ON 1-95	DETAILS 3	04.012
REV DATE REVISION DESCRIPTION	SHEET NO Plotted Date: 10/17/2013		Filename:\SB_MSH_S-13_PARAPET_JOINT_DETAIL_3.dgn			DETAILS 3	04.013



JOINT TREATMENT AT PARAPET

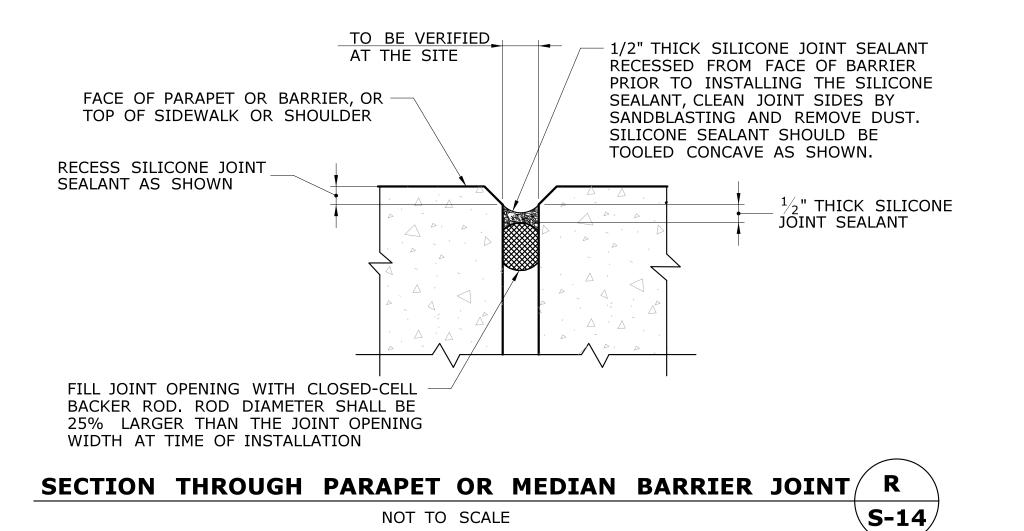
NOT TO SCALE

PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".



JOINT TREATMENT AT CONCRETE MEDIAN BARRIER AT BRIDGE NO. 01639 - SOUTHBOUND

NOT TO SCALE

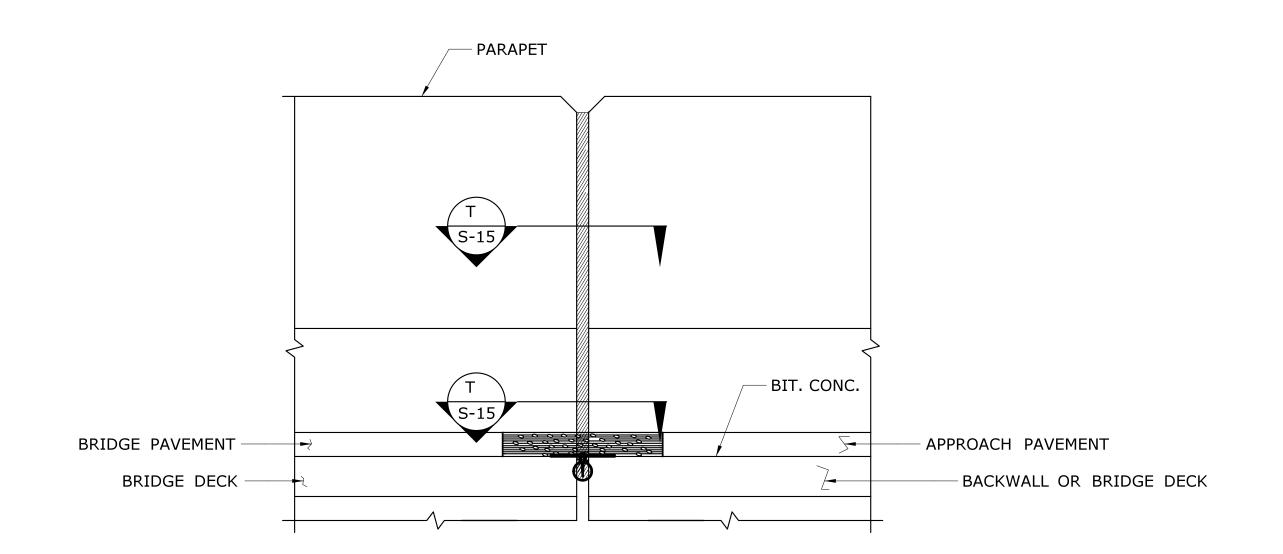


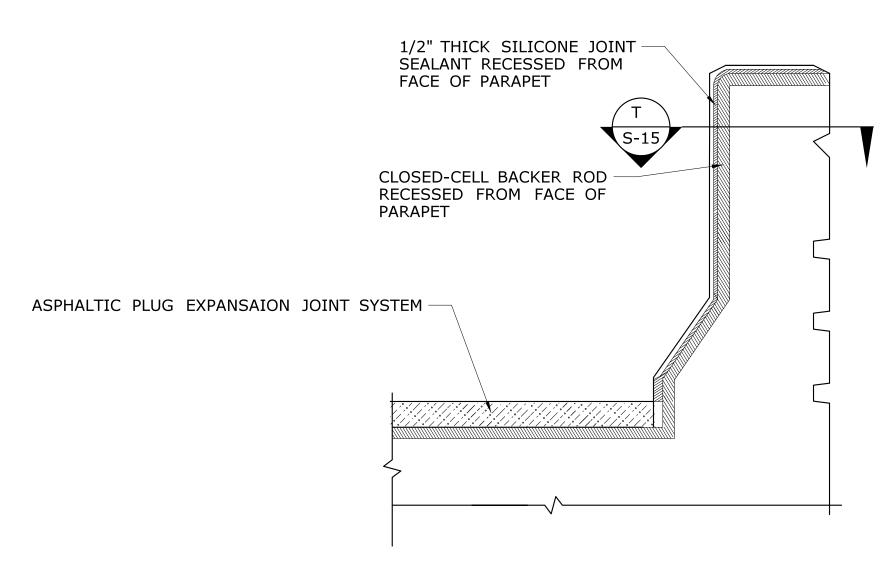
SILICONE JOINT SEALANT AND BACKER ROD DETAILS AT PARAPET AND MEDIAN BARRIER, SOUTHBOUND - BR. 01639

NOT TO SCALE

NOT TO SCALE

STATE OF CONNECTICUT THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. BU 58-329 - - -OFFICE OF ENGINEERING TOWN OF GROTON - | - | -PAVEMENT PRESERVATION S-14 _ | _ _ _ **DEPARTMENT OF TRANSPORTATION** - - -**PARAPET JOINT** SHEET NO. ON I-95 SCALE AS NOTED **DETAILS 4** 04.014 REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 10/17/2013 Filename: ...\SB_MSH_S-14_PARAPET_JOINT DETAIL_4.dgn





JOINT TREATMENT AT PARAPET

REV. DATE

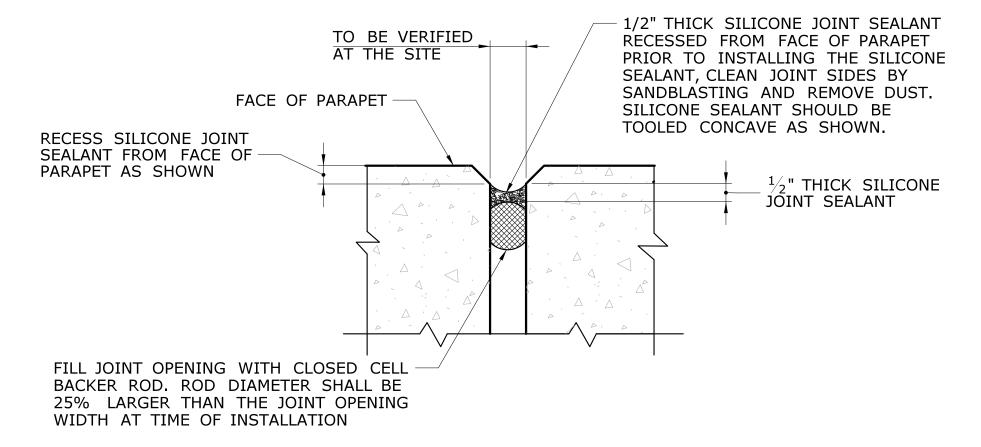
REVISION DESCRIPTION

SHEET NO. Plotted Date: 10/17/2013

NOT TO SCALE

PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".





SECTION THROUGH PARAPET OR MEDIAN BARRIER JOINT

NOT TO SCALE

∖S-15∕

SILICONE JOINT SEALANT AND BACKER ROD DETAILS IN PARAPET

NOT TO SCALE

STATE OF CONNECTICUT - - -THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. OFFICE OF ENGINEERING - | - | -PAVEMENT PRESERVATION _ - | -**DEPARTMENT OF TRANSPORTATION** - - -- - -ON I-95 SCALE AS NOTED

Filename: ...\SB_MSH_S-15_PARAPET_JOINT DETAIL_5.dgn

58-329 TOWN OF GROTON S-15 **PARAPET JOINT** SHEET NO. **DETAILS 5** 04.015